

TECHNICAL MEMORANDUM

DATE: December 3, 2025

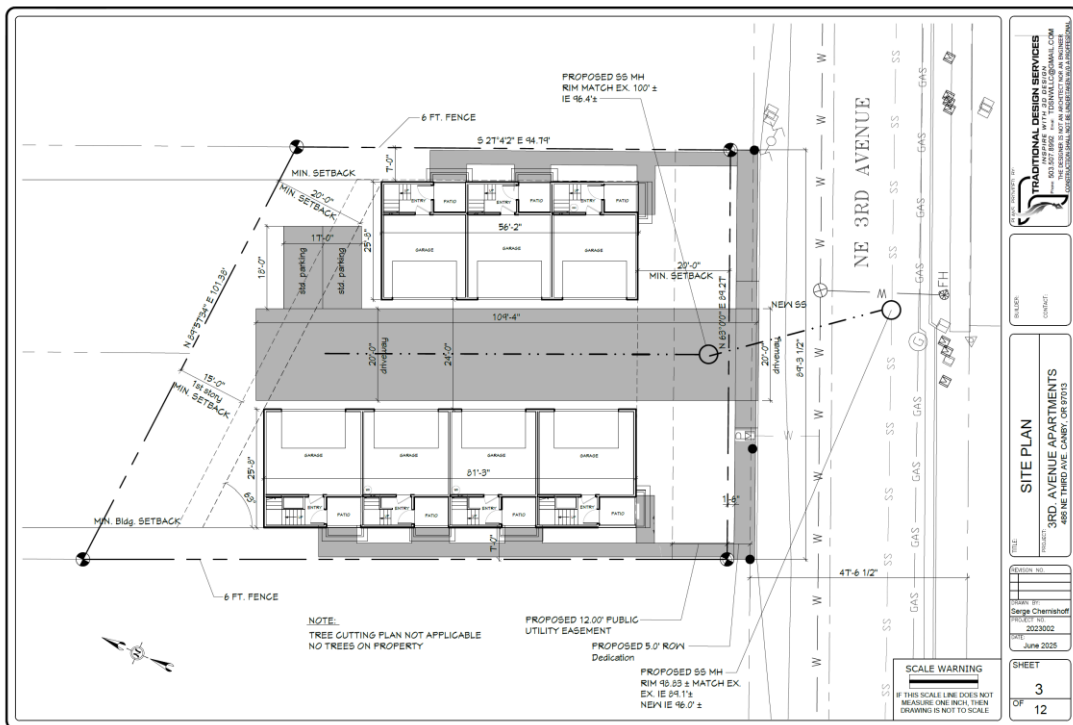
TO: Don Hardy and Brianna Addotta | City of Canby

FROM: Kevin Chewuk | DKS Associates

SUBJECT: Canby NE 3rd Ave Apartments
Transportation Analysis Findings

This memorandum summarizes the transportation impacts associated with the proposed NE 3rd Avenue Apartments. This analysis addresses the transportation analysis requirements of the City and is based on the current plan which proposes 7 attached townhouse units in two buildings, one with 4 units and another with 3 units. The proposed site is located at 486 NE 3rd Avenue on 0.25-acre lot, and the proposed use is consistent with the high density residential (R-2) zoning designation. Each unit is proposed to include garage space for 2 parked vehicles, and 2 additional on-site parking spaces are proposed. The site plan for the proposed use is shown in Figure 1.

FIGURE 1: SITE PLAN FOR THE PROPOSED NE 3RD AVE APARTMENTS



PROPOSED SITE TRIP GENERATION

The amount of new vehicle trips generated by the proposed use was estimated using the trip generation estimates based on ITE Code 215 (Single-Family Attached Housing) using the 11th Edition of the ITE Trip Generation Manual. Trip generation estimates are provided for daily, morning, and evening peak hours, and are summarized in Table 1.

The proposed use will be expected to generate 3 a.m. peak trips, 4 p.m. peak trips, and 50 daily trips. The new site generated trips are not significant enough to degrade peak hour intersection operations and an off-site traffic impact analysis is not warranted¹. The proposed use is therefore consistent with the transportation approval criteria 16.08.160.F (i.e., adopted intersection mobility standards).

TABLE 1: TRIP GENERATION FOR THE PROPOSED PROJECT

LAND USE (SIZE)	AM PEAK			PM PEAK			DAILY TRIPS
	IN	OUT	TOTAL	IN	OUT	TOTAL	
SINGLE-FAMILY ATTACHED HOUSING - ITE CODE 215 (7 UNITS)	1	2	3	2	2	4	50

NEIGHBORHOOD TRIPS

A neighborhood trip impact is triggered when a proposed site adds 30 peak trips or 300 daily trips to a residential local street². As shown in Table 1, the proposed site is expected to generate 4 or fewer peak trips, and 50 daily trips, well under this standard. The proposed site will access directly to a collector, NE 3rd Avenue, and does not have an impact on residential local streets.

PEDESTRIAN AND BICYCLE TRIPS

The number of people who choose to walk, bike or ride transit along with the distances they travel is important for assessing how well existing transportation facilities serve the needs of users. The proposed use is estimated to generate 9 new bike, walk, or transit trips during an average weekday³.

¹ Peak hour intersection operations will not be degraded by proposed developments that generate fewer than 25 a.m. and/or p.m. peak trips since these trips are distributed system wide and do not all impact a single location, including intersections and roadway segments. Proposed developments that generate fewer than 25 net new peak hour trips are deemed consistent with 16.08.160.F (i.e., adopted intersection mobility standards) and must complete a Transportation Analysis Letter (TAL).

² Canby Municipal Code 16.08.160.H. Updated July 2022.

³ Bike, walk, and transit trip share assumed to be 18 percent of vehicle trips, and is estimated based on US Census Data and the Canby travel demand model.

PROPOSED SITE ACCESS AND CIRCULATION

SITE ACCESS

The site proposes adding a driveway from NE 3rd Avenue connecting the new on-site parking drive aisle, as shown in Figure 1. The proposed access will be 20 feet wide, which complies with City access width requirements for residential uses with 3-19 units⁴, with the proposed driveway complying with this standard.

ACCESS SPACING

The City of Canby has jurisdiction over NE 3rd Avenue along the frontage of the proposed site and applies a functional classification of "Collector" to it. City standards require that access along a Collector must be a minimum of 100 feet from roadways or other driveways along the same side of the street⁵. The proposed driveway to NE 3rd Avenue will be approximately 100 feet from the nearest existing adjacent driveway, complying with the spacing standard.

SIGHT DISTANCE

The sight triangle at intersections should be clear of objects (large signs, landscaping, parked cars, etc.) that could potentially limit vehicle sight distance. In addition, all proposed access should meet AASHTO sight distance requirements as measured from 15 feet back from the edge of pavement⁶.

The proposed driveway to NE 3rd Avenue requires a minimum of 280 feet of sight distance based on a 25-mph speed. A preliminary sight distance evaluation from the approximate location of the new driveway indicates that there would be expected sight distance past N Locust Street (about 250 feet) looking to the southwest, suggesting adequate sight distance. Sight distance was not evaluated looking to the northeast since NE 3rd Avenue is a one-way street heading in that direction. Prior to the City conducting the final inspection, sight distance will need to be verified, documented, and stamped by a registered professional Civil or Traffic Engineer licensed in the State of Oregon.

SITE FRONTAGE

The proposed site has frontage along NE 3rd Avenue, and as documented earlier City of Canby applies a functional classification of "Collector" to it. The street is improved to meet the City's cross-section standard and has an existing paved width of approximately 28 feet, with a one-way travel lane, on-street parking on the north side and a bike lane on the south side. It also includes

⁴ Canby Municipal Code 16.10.070.B.7. Updated October 2019.

⁵ Canby Municipal Code 16.46.030. Updated October 2019.

⁶ AASHTO – *Geometric Design of Highways and Streets*, 7th edition, 2018.

an existing sidewalk along the frontage of the proposed site. The existing roadway can adequately accommodate the additional vehicle, pedestrian, and bicycle traffic expected.

INTERNAL SITE CIRCULATION

The proposed site plan (shown earlier in Figure 1) shows the site is proposing one driveway to NE 3rd Avenue. The driveway will be paved with a 20 feet drive aisle and provide access to the on-site parking areas for vehicles and bicycles. The proposed driveway access can adequately accommodate vehicle and bicycle circulation to NE 3rd Avenue and internally within the site.

The site plan also includes proposed sidewalk connections from the entrance of each dwelling unit and the parking area to NE 3rd Avenue, complying with City pedestrian access requirements for residential uses with 3-19 units^{7 8}.

APPROVAL CRITERIA AND LIVABILITY MEASURES

The following sections summarize how the proposed site adequately addresses the transportation approval criteria and the livability measures for neighborhood traffic and pedestrian and bicycle circulation.

TRANSPORTATION APPROVAL CRITERIA

The Canby Municipal Code 16.08.170 includes transportation approval criteria that each proposed development must satisfy. This includes criteria B, D, E, and F, as summarized below. While Criteria A, C and E.3 are not transportation related criteria, they are still applicable for approval. See the respective documents or plans for more details on how this proposed development meets Criteria A, C and E.3, if applicable for this land use application.

A. ADEQUATE STREET DRAINAGE, AS DETERMINED BY THE CITY.

Non-transportation related criteria. See respective project documents/plans for information.

B. SAFE ACCESS AND CLEAR VISION AT INTERSECTIONS, AS DETERMINED BY THE CITY.

The proposed driveway to NE 3rd Avenue will be approximately 100 feet from the nearest existing adjacent driveway, complying with the City spacing standard.

Prior to the City conducting the final inspection, sight distance will need to be verified, documented, and stamped by a registered professional Civil or Traffic Engineer licensed in the State of Oregon. A preliminary sight distance evaluation from the approximate location of the

⁷ Canby Municipal Code 16.10.070.B.5. Retrieved August 2021.

⁸ Canby Municipal Code 16.10.070.B.7. Updated October 2019.

new driveway suggests that adequate sight distance is expected when looking to the southwest. Sight distance was not evaluated looking to the northeast since NE 3rd Avenue is a one-way street heading in that direction.

C. ADEQUATE PUBLIC UTILITIES, AS DETERMINED BY THE CITY.

Non-transportation related criteria. See respective project documents/plans for information.

D. ACCESS ONTO A PUBLIC STREET WITH THE MINIMUM PAVED WIDTHS AS STATED IN SUBSECTION E BELOW.

The site proposes adding a driveway from NE 3rd Avenue connecting the new on-site parking drive aisle. The proposed 20-foot access complies with City access width requirements.

The site includes proposed sidewalk connections from the entrance of each dwelling unit and the parking area to NE 3rd Avenue, complying with City pedestrian access requirements.

E. ADEQUATE FRONTAGE IMPROVEMENTS AS FOLLOWS:

- 1. For local streets and neighborhood connectors, a minimum paved width of 16 feet along the site's frontage.**
- 2. For collector and arterial streets, a minimum paved width of 20 feet along the site's frontage.**
- 3. For all streets, a minimum horizontal right-of-way clearance of 20 feet along the site's frontage.**

The proposed site has frontage along NE 3rd Avenue, which is classified as a Collector street and subject to Subsection E.2. The street is improved to meet the City's cross-section standard and has an existing paved width of approximately 28 feet, with a one-way travel lane, on-street parking on the north side and a bike lane on the south side.

F. COMPLIANCE WITH MOBILITY STANDARDS IDENTIFIED IN THE TSP. IF A MOBILITY DEFICIENCY ALREADY EXISTS, THE DEVELOPMENT SHALL NOT CREATE FURTHER DEFICIENCIES.

The proposed use will be expected to generate no more than 4 new peak hour trips, and 50 daily trips. All intersections and segments will see fewer than 4 new peak hour trips, and therefore met criteria for a TAL level of analysis. Peak hour intersection operations will not be degraded by proposed developments that generate fewer than 25 a.m. and/or p.m. peak trips since these trips are distributed system wide and do not all impact a single location, including intersections and roadway segments. Proposed developments that meet the TAL criteria are deemed consistent with Subsection F.

LIVABILITY CRITERIA

In addition, each project must comply with livability measures for neighborhood traffic and pedestrian and bicycle circulation. A summary is provided below for the proposed project.

NEIGHBORHOOD TRAFFIC

The proposed use will access directly to a collector, NE 3rd Avenue, and does not have an impact on residential local streets.

PEDESTRIAN AND BICYCLE CIRCULATION

The proposed site is estimated to generate 9 new bike, walk, or transit trips during an average weekday. It has frontage along NE 3rd Avenue, which is improved to meet the City's cross-section standard, with a bike lane on the south side and an existing sidewalk.